

## Section V: Design Guidelines



Within the Hunt Valley / Timonium area, there are many examples of good design quality. The quality of these developments, particularly at the Hunt Valley Business community, has promoted an image that has become important in attracting businesses to the area. Design quality controls can help assure that the entire HV/T area will continue to provide an attractive, desirable environment in which to work, live, and shop, and to help maintain its status in the County as a major corporate economic development area. The Hunt Valley Business Community was developed with deed covenants to assure a high quality of design. The guidelines proposed here reflect those major design elements which will help direct the retail, office and industrial development in the entire HV/T area toward consistent design quality.

The guidelines also address the needs of pedestrian and transit users. Employees using transit frequently find no walks to their buildings, or must walk through

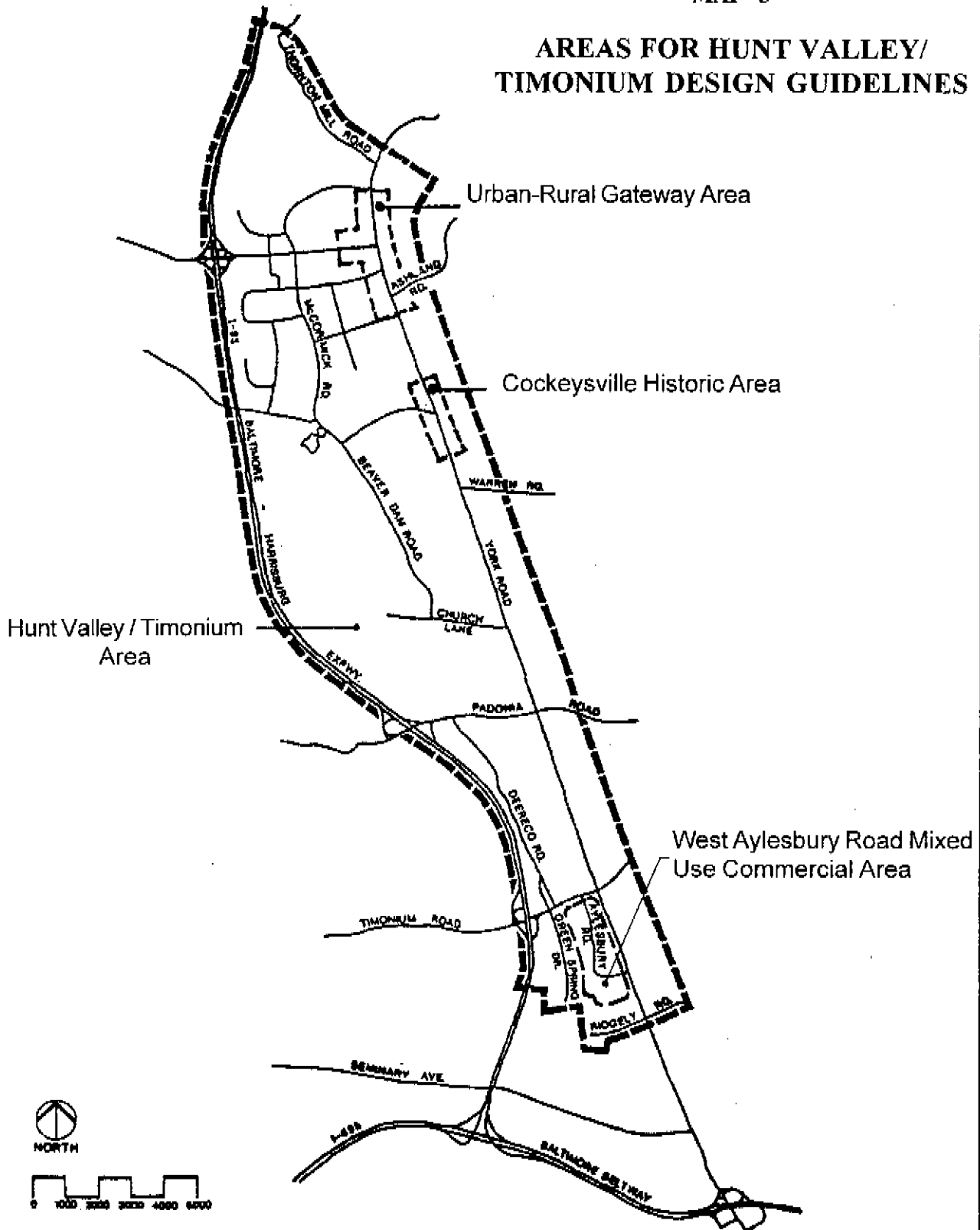
expansive parking lots to reach the entrance. Pedestrian facilities which connect adjacent uses, or link businesses to restaurants and other employee services, will reduce the need for auto travel and provide an important amenity.

Additional guidelines that pertain to the Urban/Rural Gateway Area, the Cockeysville Historic Area and the West Aylesbury Road Mixed Use Area are also included to reflect their special natures. Other sections of the Comprehensive Manual of Development Policies may also apply to development in HV/T, particularly the sections entitled Office Guidelines, Commercial Corridors, and Mixed Use Development.

Adoption of the Hunt Valley/Timonium Master Plan by the Planning Board and by the County Council will constitute the adoption of the guidelines contained in this section (with any amendments) into the Baltimore County Comprehensive Manual of Development Policies.

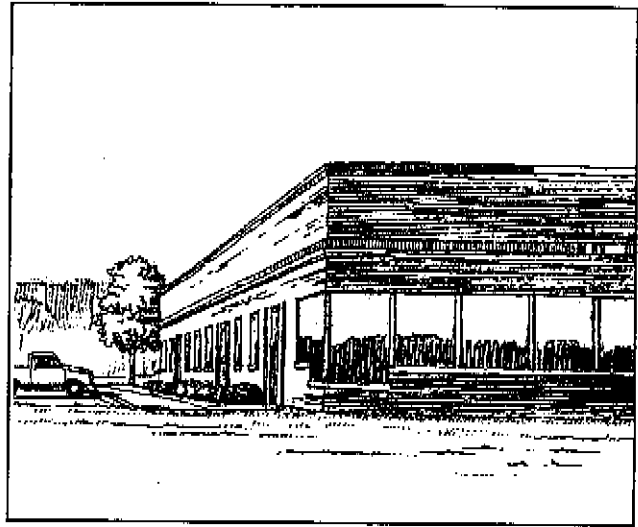
MAP 5

AREAS FOR HUNT VALLEY/  
TIMONIUM DESIGN GUIDELINES

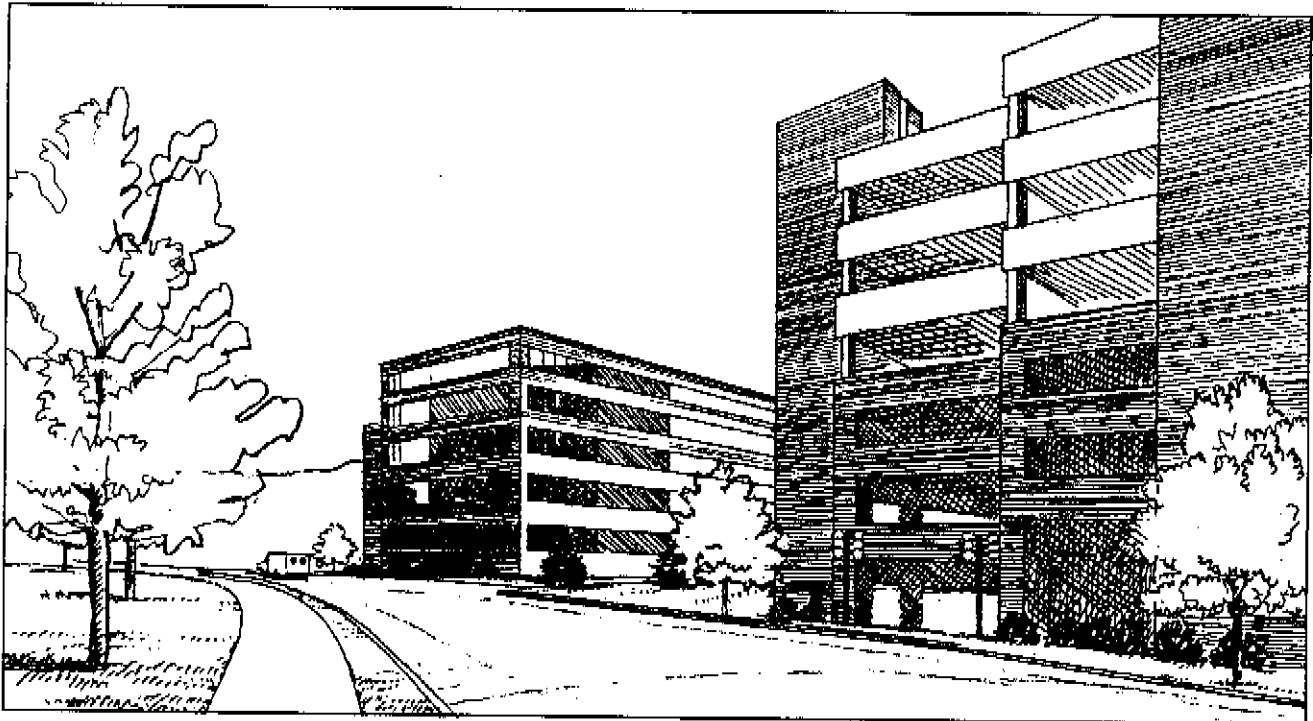


## BUILDINGS

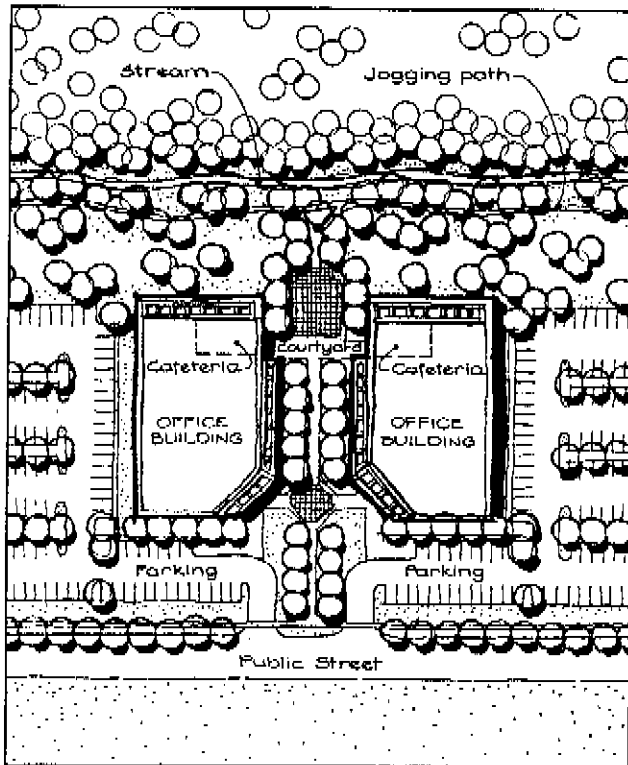
- Vary and articulate building elements to visually and dimensionally interrupt the bulk of the building. Highlighting entranceways and establishing focal points are among the ways to achieve this guideline.
- Use a consistent architectural design and finish on all facades which are visible from public rights-of-way.
- Finish the sides of buildings which are visible from public rights-of-way with a high quality material such as brick or decorative concrete block.
- Treat above-ground parking structures with a quality architectural finish such as decorative concrete block or brick, visually compatible with the buildings they support.
- The height and scale of buildings should be compatible with the surrounding area.



*Consistent architectural detailing and finishes create more attractive buildings.*



*Use of compatible materials and detailing on parking structures creates a unified visual image.*



*Clustered buildings oriented toward natural features with shared public or employee open space areas provide effective integration and use of open space.*

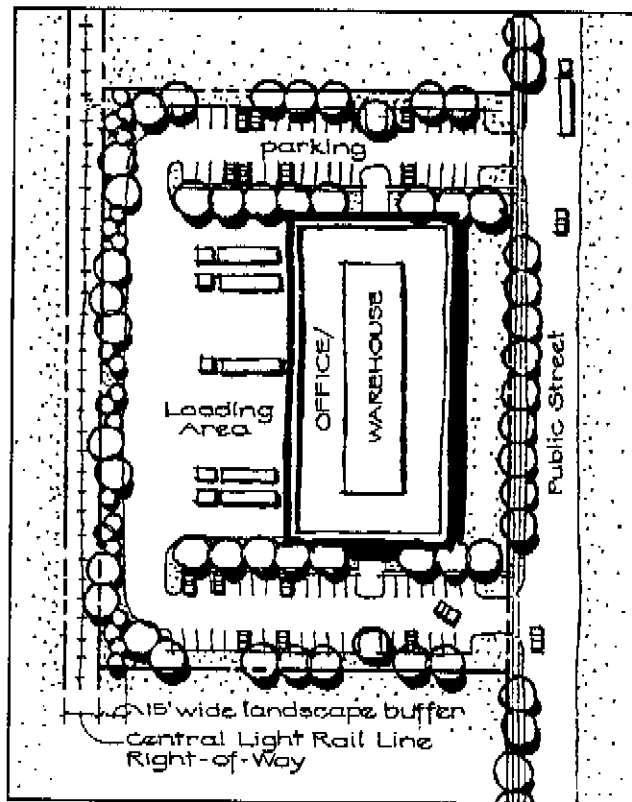
## OPEN SPACE

Design an open space network as an integral part of the overall site plan. Incorporate existing streams and required forest buffers into the open space network.

- Include amenities in the open space network for active or passive use by employees such as picnic tables, benches and other facilities for sitting, eating, meeting or recreational activities. Where feasible, take advantage of the space between buildings to include a courtyard or site feature.
- Provide access to, and views of, the open space network. Where possible, locate appropriate building functions towards the view (c.g., the cafeteria instead of the loading area).

## WATER QUALITY MANAGEMENT

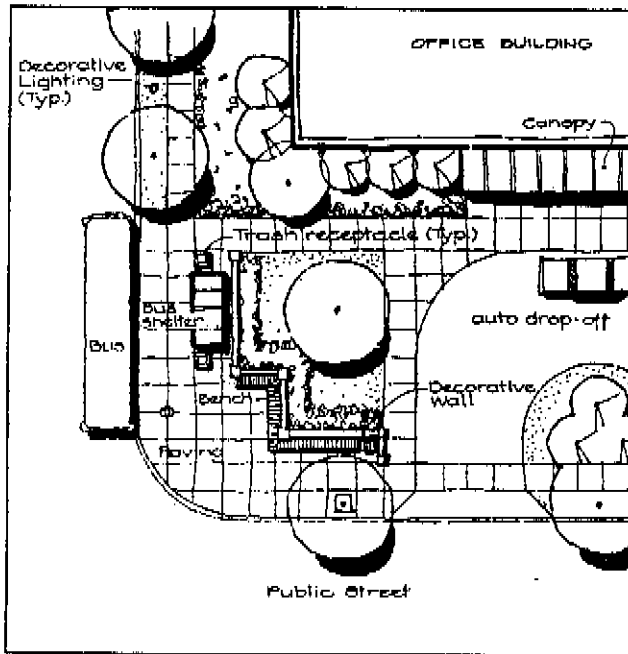
- For new developments and redevelopments, use "Low Impact Development" (LID) techniques, where feasible, to infiltrate or detain stormwater runoff. These techniques include bioretention; open, vegetated drainage swales; curbless roadways and other paved areas; and redirected or lengthened routing pathways for stormwater flows to maximize pollutant removal and flood peak attenuation.
- Perform periodic "street cleaning" operations on privately owned paved roadways and parking areas to minimize the build-up of surface pollutants.



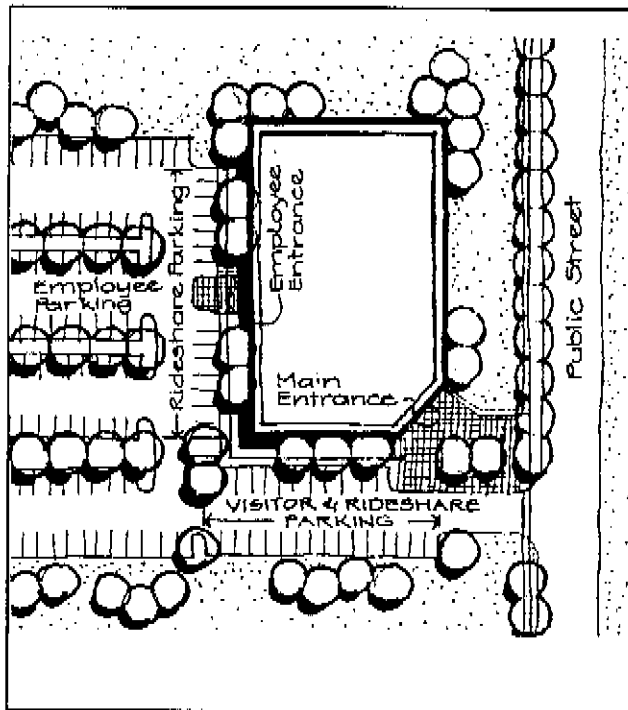
*A landscaped/fenced buffer between the light rail line and parking, service and storage areas will help provide a pleasant environment for transit passengers.*

## SCREENING AND BUFFERING

- For developments adjacent to I-83, screen parking lots, parking structures and service areas from the freeway with a 50 foot minimum landscaped buffer, as measured from the I-83 paving edge. Existing vegetation within the right-of-way may be incorporated as part of the landscape design as long as at least 25 feet of the landscaped buffer occurs on the building site.
- Enclose or screen mechanical equipment, both roof- and ground-mounted, in a manner which appears as an integral part of the architectural or site design.
- Where possible, orient buildings so that loading and service areas do not face public streets.
- Screen outdoor storage areas from view of public rights-of-way.
- Provide a 15-foot wide landscaped and/or fenced buffer between the light rail line right-of-way and parking, service and storage areas.
- Where a fence is used as a screen, landscape any side in the public view, and landscape areas adjacent to residential uses or zones.



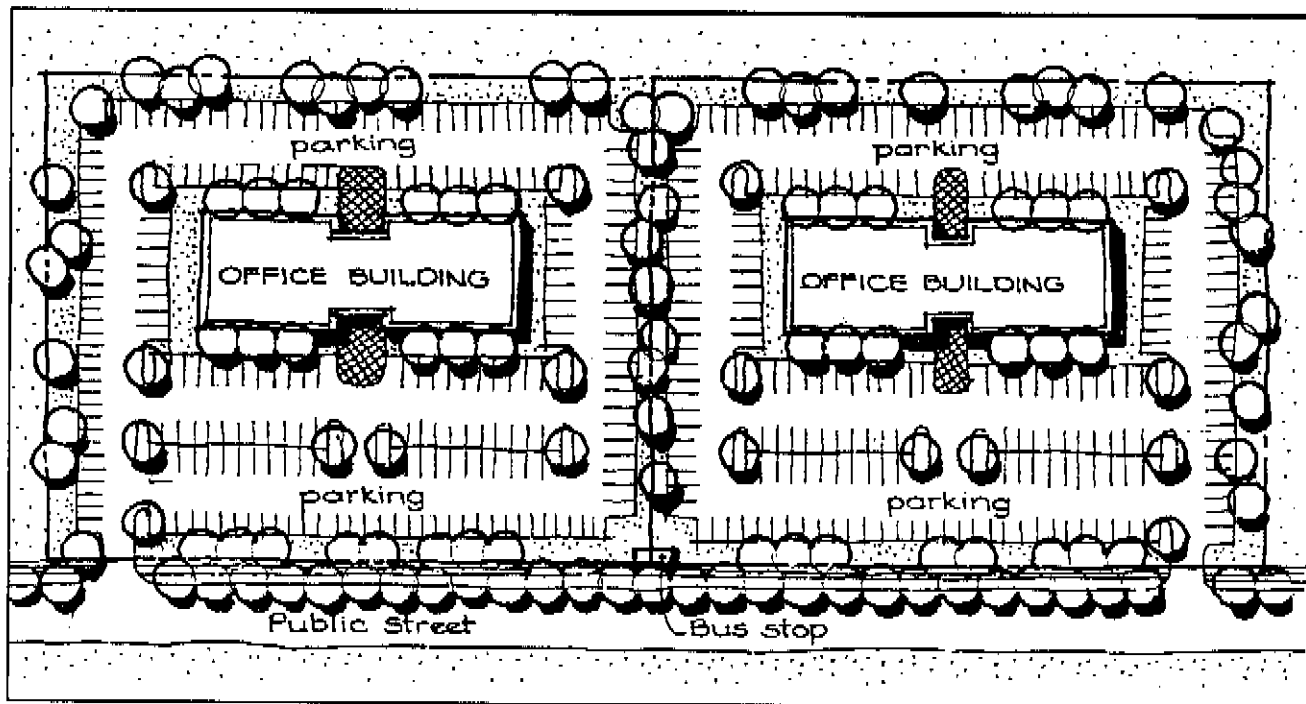
*Transit stops should be attractive pedestrian spaces designed as part of the building entrance.*



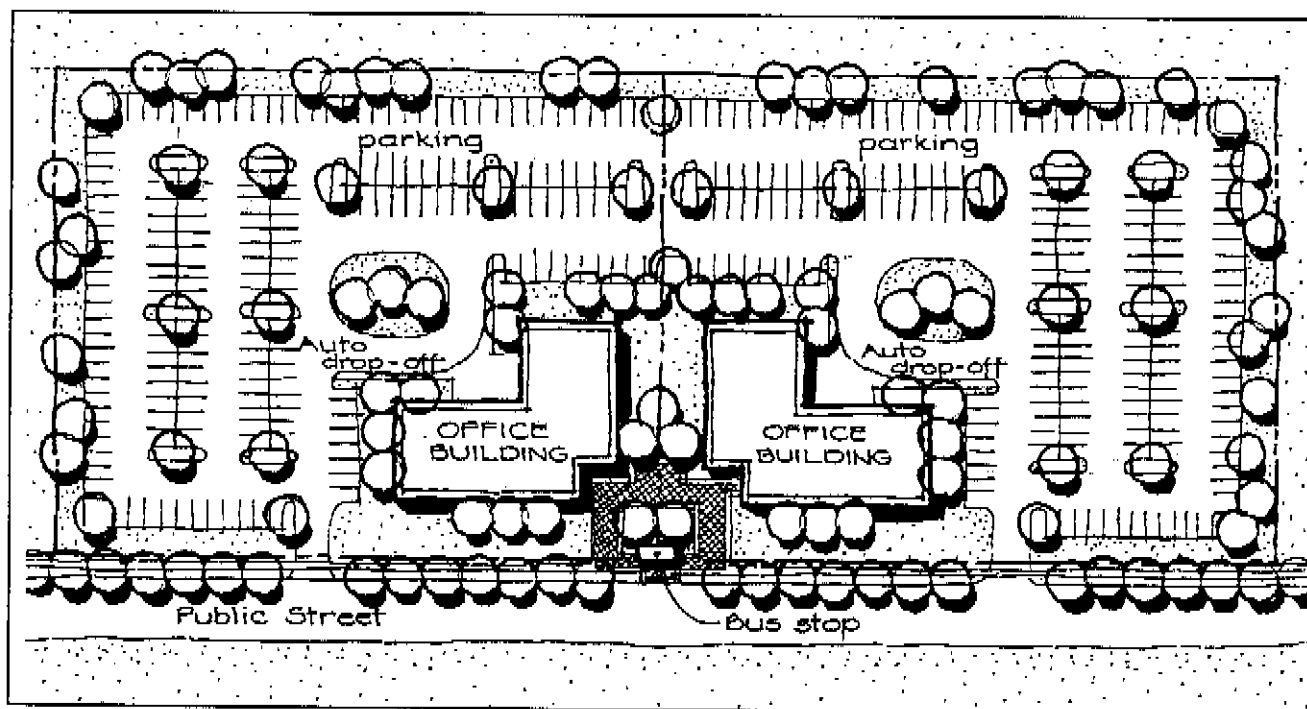
*Locate parking for rideshare vehicles near the building entrance.*

## PEDESTRIAN- AND TRANSIT-ORIENTED DESIGN

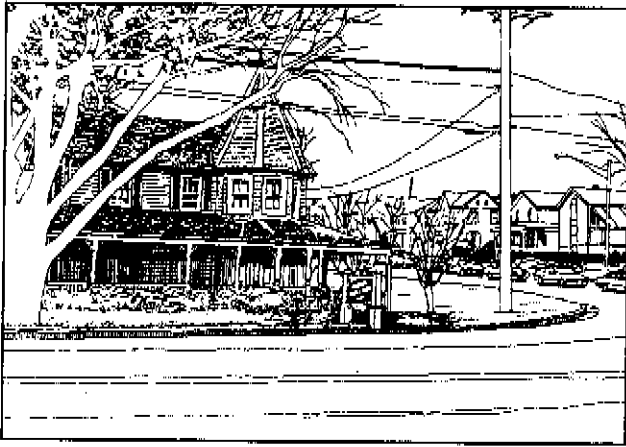
- Provide sidewalks to connect buildings to each other, the street, and transit stops. Sidewalks are to be paved, lit and landscaped without hindering visibility.
- Provide street trees adjacent to public rights-of-way to help separate pedestrian and motorized traffic.
- Provide direct pedestrian access from the street to the building entrance with minimal crossings through parking lots and drives. Limit parking to one bay width (maximum) between the entrance and the street.
- Orient the building towards the public transportation facility rather than the parking lot. Arrange the building on the site to reduce the walking distance to the transit stop.
- When adjacent to a transit stop, incorporate benches, trash receptacles with lids, lighting, shelters, and where appropriate, bus pull-outs, into the overall development.
- Provide preferential parking close to the building for rideshare vehicles.



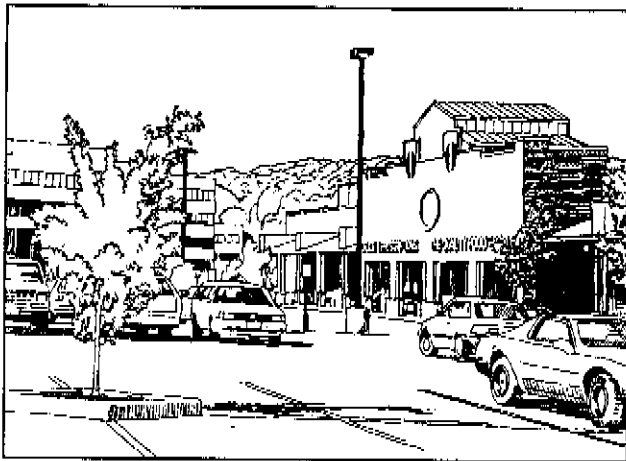
*This site layout favors access by private auto. Transit users must walk through the parking lot to reach the building entrance.*



*This site design is "transit friendly." Transit users have direct access to the building. Clustering buildings around a central pedestrian space also encourages pedestrian access and shortens walking distances.*



*The existing rural center character of the gateway should be reflected by new development.*

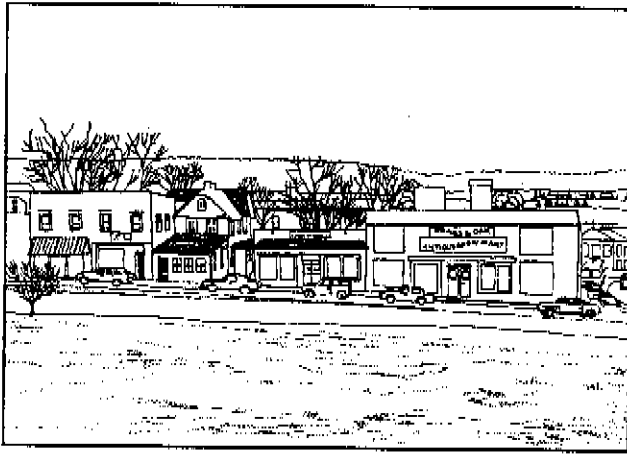


*New development should incorporate design elements, such as roof pitch, or other details and materials, that reflect, or are compatible with, the gateway character.*

## URBAN-RURAL GATEWAY

The northern end of York Road within the Hunt Valley/Timonium area coincides with the Baltimore County Urban-Rural Demarcation Line. It is an area which functions as a gateway, and presents an opportunity for fostering development which reflects a consistent theme, and presents a unified and attractive appearance. The existing development pattern of older "quaint" office uses, in combination with the newer brick office and retail uses, presents a sense of transition between urban and rural centers. It is this attractive, transitional character that should be emphasized.

- Within the gateway area, use compatible and consistent architectural styles and materials to provide a high quality visual appearance. Structures utilizing red brick with green metal standing seam roofing are compatible, as well as those which, through their scale, materials and detailing, complement the existing residential office development.
- The design and materials used for signage and lighting should improve the visual continuity of the area; they should not serve as a distraction.
- The design and materials used for signage and lighting should complement and be similar to those used for the building.
- Lighting and signage should be in scale with the building, and where possible, integrated into the building design rather than freestanding.



*New development in the Cockeysville Historic Area should reflect a historic theme.*

## COCKEYSVILLE HISTORIC AREA

Redevelopment of the Cockeysville Historic Area provides an opportunity to preserve and enhance a unique element within the Hunt Valley/Timonium Area. Uses which focus on antique and arts/crafts sales, and development/redevelopment which maintains the historic theme, will add to the vibrancy of the community.

- Within the Cockeysville Historic Area, use compatible and consistent architectural styles and materials which reflect its historic appearance and provide high visual quality. Structures utilizing red brick, and green metal standing seam roofing are compatible, as well as those which, through their scale, materials and detailing, complement the existing historic development.
- The design and materials used for signage and lighting should improve the visual continuity of the area; they should not serve as a distraction.
- The design and materials used for signage and lighting should complement and be similar to those used for the building.
- Lighting and signage should be in scale with the building, and where possible, integrated into the building design rather than freestanding.

## WEST AYLESBURY ROAD MIXED USE AREA

**INTENT:** To encourage the use of the PUD-C process in the West Aylesbury Road area for redevelopment as a mixed use retail/office/light industrial center. Requests for retail use which meet the following guidelines should be considered an appropriate land use.

All sites developed in this area under any development process should comply with these, and other guidelines where applicable, to ensure design consistency and unity, and be reviewed for conformance by the Design Review Panel.

### GUIDELINES:

- No single retail use shall exceed 80,000 s.f., and all the retail uses on the site shall not exceed a floor to area ratio of 0.33.
- A 15-foot wide space shall be provided between the roadway curb and parking to accommodate a 6-foot wide sidewalk, meandering where appropriate, and street trees.
- Six-foot wide sidewalks shall be provided along all public roads.
- Street trees shall be provided along all public roads at a minimum of 60' on center.
- Coordinated pedestrian lighting shall be provided along all public roads.
- Attractive, coordinated crosswalks shall be provided at intersections and wherever a walkway crosses a vehicular drive.
- Provisions shall be made for internal pedestrian circulation.
- Buildings and their entrances shall be located in close proximity to West Aylesbury Road, or to Business Park Drive where applicable, and provide direct pedestrian access from the street.
- Finish the sides of the buildings with a high quality material such as brick or decorative concrete block.
- Articulate the building facades to visually interrupt the bulk of the building.
- Use a consistent architectural design and finish on all facades of the building which are visible to the public, compatible with the West Aylesbury Road Center "theme."
- Enclose or screen dumpsters and mechanical equipment, both roof- and ground-mounted, in a manner which appears as an integral part of the architectural or site design.
- Signage shall be building- or ground-mounted.



*The West Aylesbury Road area is in a state of transition, with many properties ripe for redevelopment. Design guidelines will help produce an attractive, pedestrian-oriented environment.*